

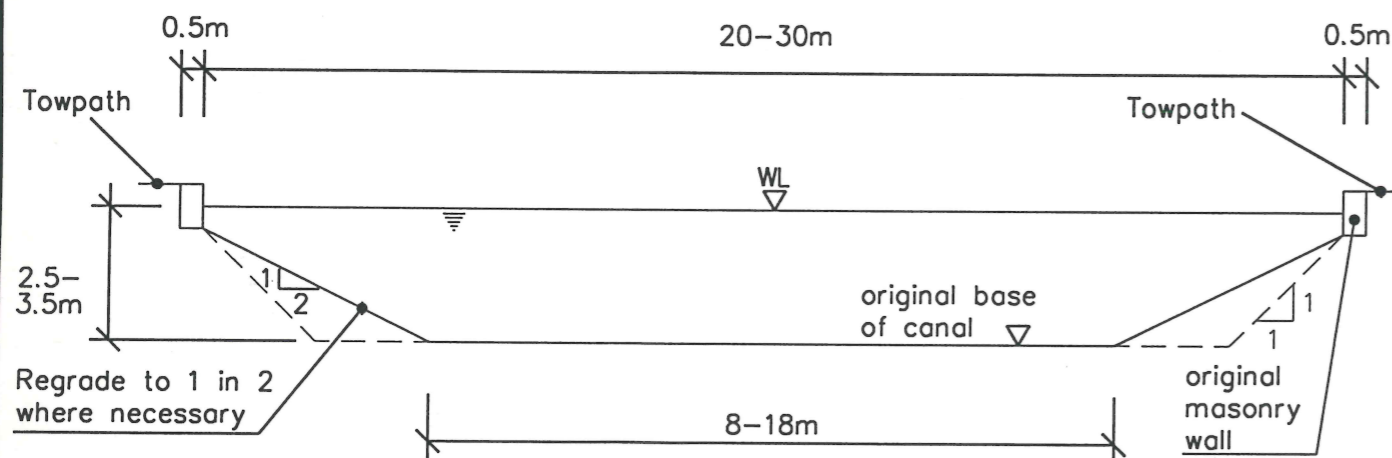
SECTION No. 1

WIDNES LOCK TO SPIKE ISLAND BRIDGE

(CH 0-600m)

DESCRIPTION - Spike Island marina. Used by river craft and is an attraction in itself. Spike Island marina is a working marina and will remain an asset regardless of the canal restoration. Therefore restoration to provide mooring on both banks is proposed and use of the original canal construction materials to retain its appeal.

RESTORED SECTION -



RESTORATION - Remove silt as necessary to give at least 2m navigable depth.
Remove 'Ruth Bate' for restoration in dry dock.
Water to be lowered in sections to allow wall repairs.
Dismantle and rebuild masonry wall.

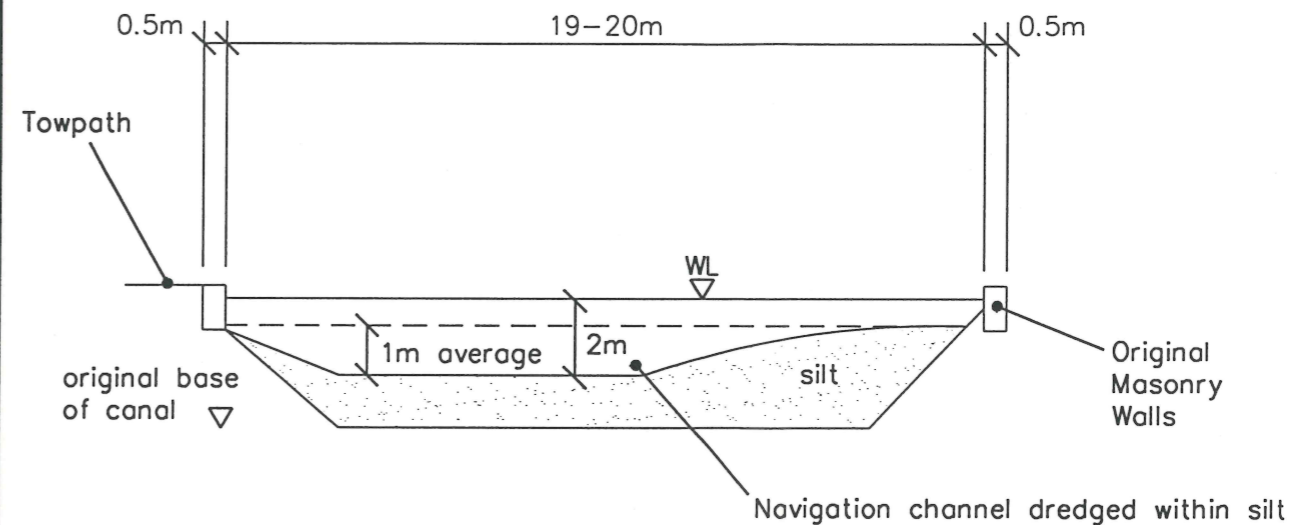
SECTION No. 2

SPIKE ISLAND BRIDGE TO CUERDLEY MARSH
CUERDLEY MARSH TO FIDDLER'S FERRY YACHT HAVEN
PENKETH BRIDGE TO MEYERS SWING BRIDGE

(CH 600-3,180m)
(CH 3,570-5,440m)
(CH 6,355-7,755m)

DESCRIPTION - 'In-Water' sections which are heavily silted and require a navigable depth of 2m.

RESTORED SECTION -



RESTORATION - Canal to remain in water during work.
Dredge canal silt to create a navigable channel.
Excess silt removed from site for disposal.

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Project **ST. HELENS CANAL RESTORATION**

Title **CROSS SECTIONS**

Scale	Drawn	Checked	Authorised
N.T.S.	DTH	JMH	
	Date 2.96	Date 2.96	Date

Drawing Number **AY2311 / 120 / 850 / 051**

Rev

A3

Purpose of issue	Auth	Date	Rev	Description	By	Date	Chk'd	Auth

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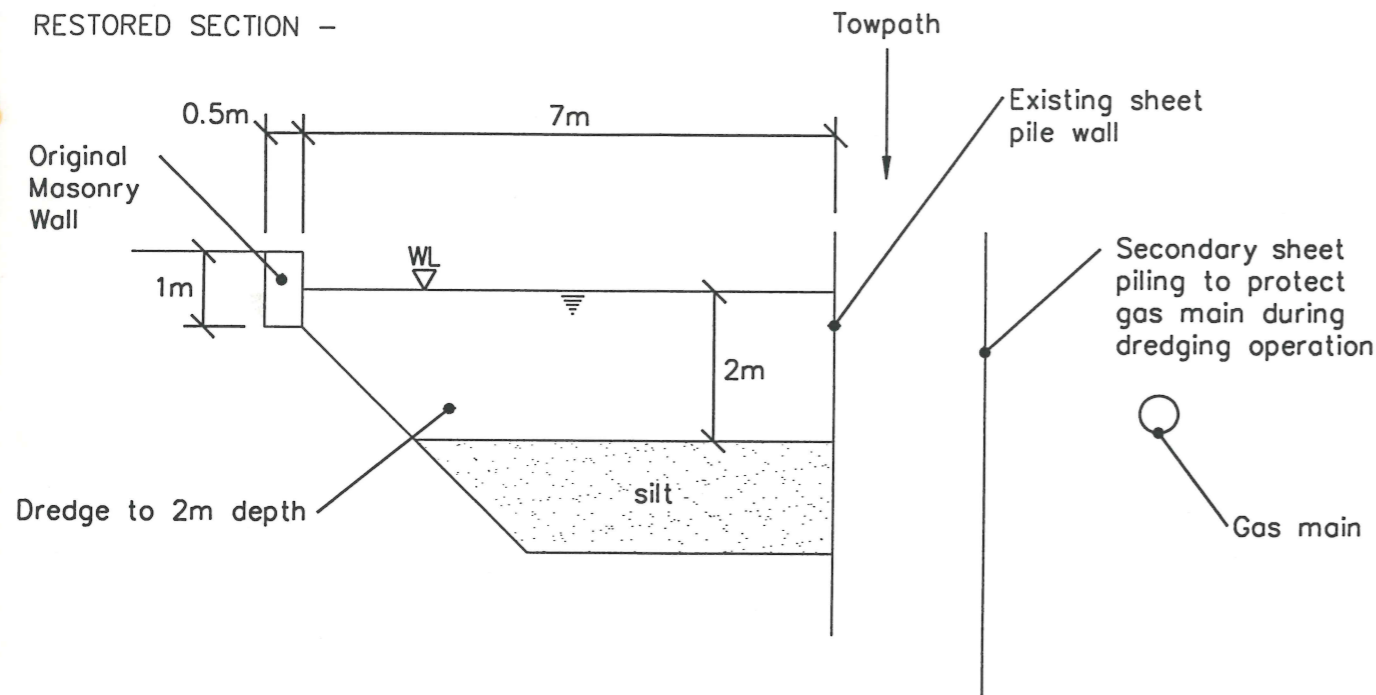
SECTION No. 3

CUERDLEY MARSH

(CH 3,180-3,570m)

DESCRIPTION - Canal has been partly infilled to protect a gas main in the towpath, which is subject to movement. Channel itself is heavily silted.

RESTORED SECTION -



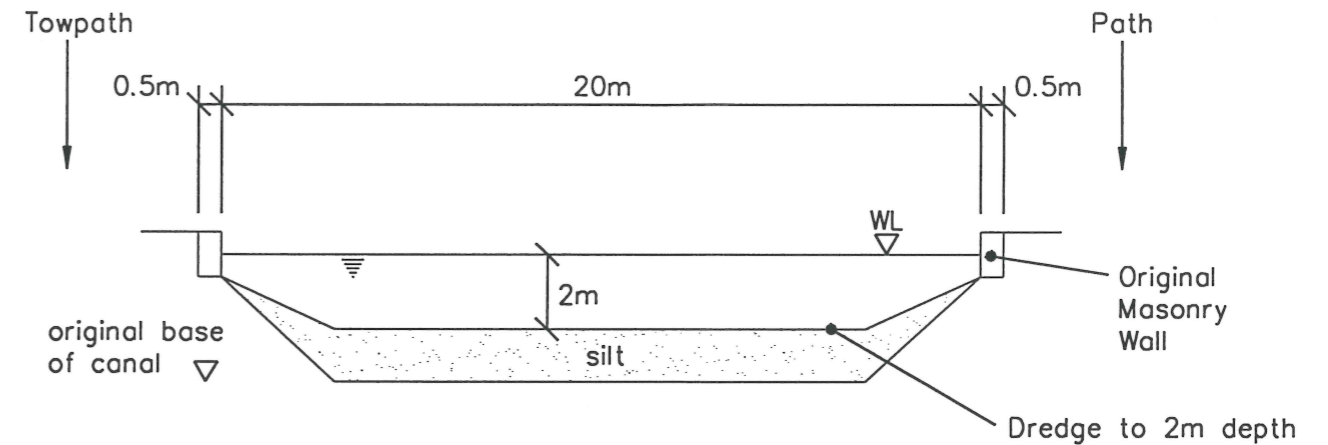
RESTORATION - Install secondary sheet piling to protect gas main prior to dredging of canal channel.
Dredge silt and remove for disposal.
Canal to remain 'In-water' during dredging.

SECTION No. 4

FIDDLERS FERRY YACHT HAVEN TO PENKETH BRIDGE (CH 5,440-6,355m)
MEYERS SWING BRIDGE TO WARRINGTON/WIDNES RAILWAY (CH 7,775-8,035m)

DESCRIPTION - Mooring areas requiring a 2m navigable depth, and mooring on two sides.

RESTORED SECTION -



RESTORATION - Dredge canal channel to 2m depth.
Canal to remain 'In-water' during work.
Remove silt and dispose.
Remedial work to canal masonry as necessary.

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Drawing Number **AY2311 / 120 / 850 / 052**

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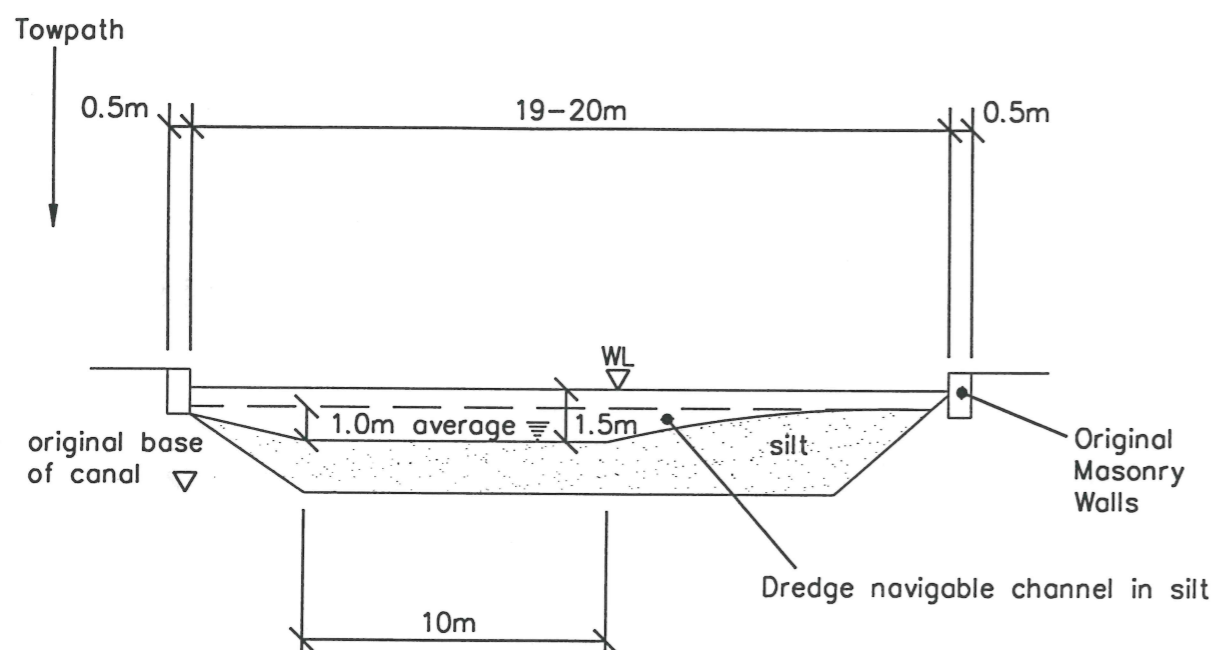
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SECTION No. 5

WARRINGTON/WIDNES RAILWAY TO BEWSEY SWING BRIDGE (CH 8,035-10,280m)
 NEWTON BROOK TO BRADLEY LOCK (CH 14,635-16,375m)

DESCRIPTION - 'In-Water' section requiring a navigable depth of 1.5m.

RESTORED SECTION -



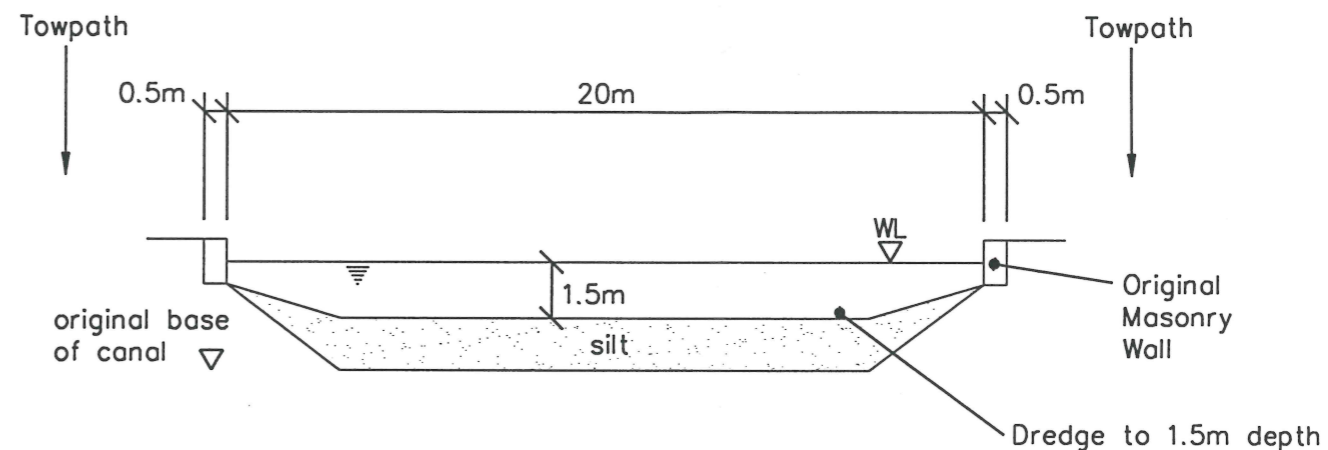
RESTORATION - Dredge 1.5m deep navigable channel in silt.
 Canal to remain 'in-water' during work.
 Remove excess silt and dispose.
 Possibility of planting berm on non-towpath side.

SECTION No. 6

BEWSEY SWING BRIDGE TO BEWSEY LOCK (CH 10,280-10,460m)

DESCRIPTION - Mooring areas requiring a 1.5m navigable depth, and mooring on two sides.

RESTORED SECTION -



RESTORATION - Dredge canal channel to 1.5m depth.
 Canal to remain 'in-water' during work.
 Remove silt and dispose.

AutoCAD File Ref. E:\Y2311\030\053.DWG

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Scale	Drawn	Checked	Authorised
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Drawing Number
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Rev

A3

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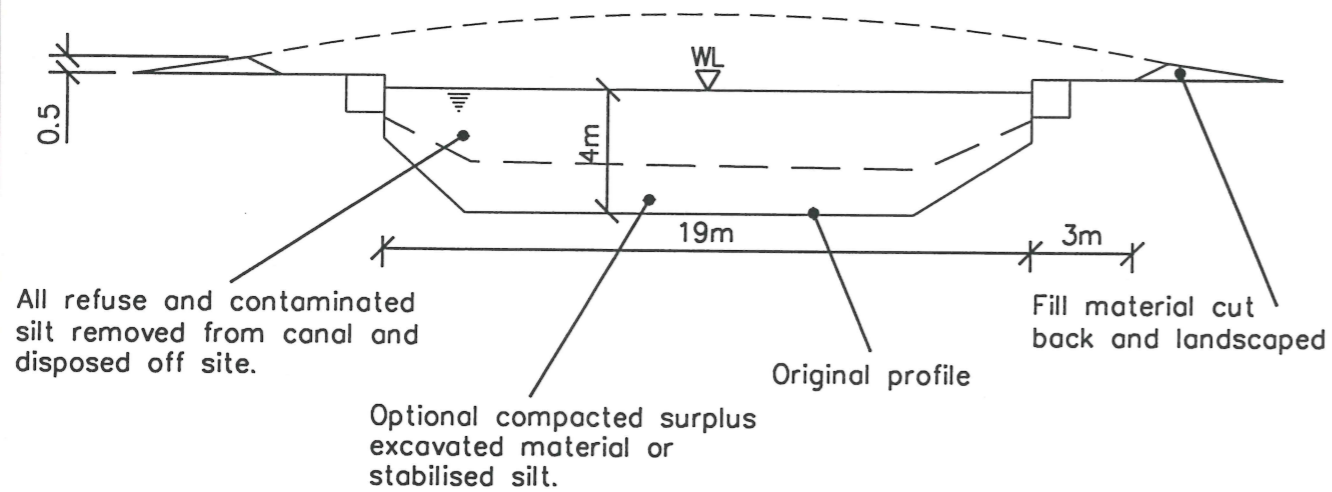
SECTION No. 7

WINWICK LOCK TO NEWTON BROOK

(CH 12,950-14,635m)

DESCRIPTION - Canal infilled with domestic refuse.

RESTORED SECTION -



RESTORATION - Excavate channel and dispose of refuse and contaminated material. Line excavation with HDPE and provide concrete/armater protection. Provide gabion wall to allow mooring.

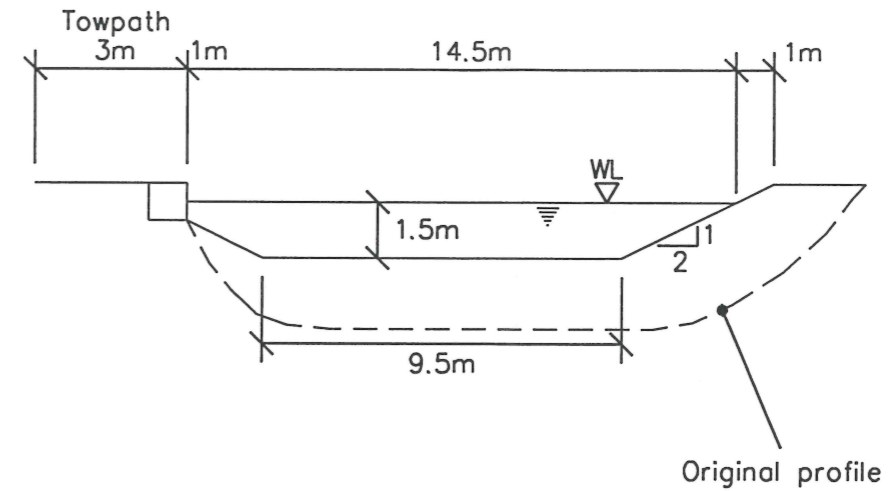
SECTION No. 8

BRADLEY LOCK TO NEWTON COMMON LOCK
NEWTON COMMON LOCK TO ENGINE LOCK

(CH 16,375-17,375m)
(CH 17,375-19,795m)

DESCRIPTION - Sections infilled with inert material over contaminated silt.

RESTORED SECTION -



RESTORATION - Excavate channel and dispose of fill. Line excavation with HDPE and provide concrete/armater protection. Provide gabion wall to allow mooring.

N.B. Between Bradley Lock and Newton Common Lock walls on both sides to be reinstated or gabion walls faced with stone to recreate original appearance.

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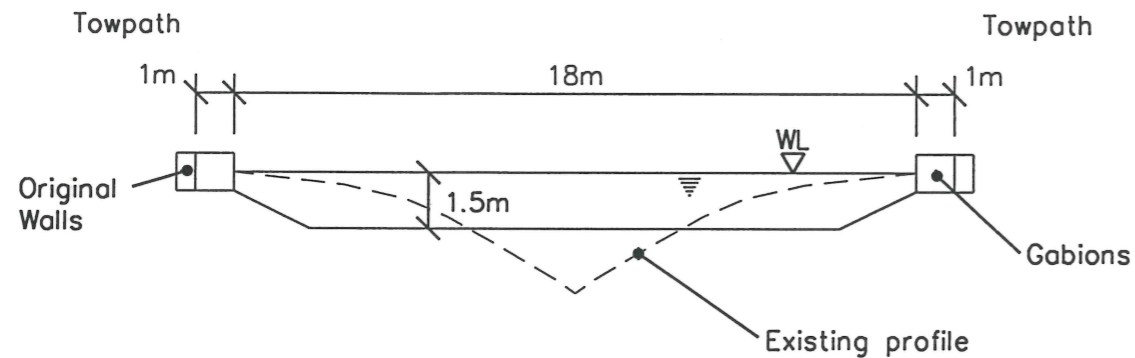
SECTION No. 9

ENGINE LOCK TO OLD DOUBLE LOCK

(CH19795-20695m)

DESCRIPTION - Black Brook flows within the original channel of the St. Helens Canal, and parallel to Clipsey Brook at this point. Canal is heavily silted and overgrown, but original canal masonry is visible on both banks, and its condition in places is poor. Utilize existing canal width but abandon walling as to expensive to restore/replace.(Unless undertaken by voluntary sector.)

RESTORED SECTION -



RESTORATION - Divert Black Brook into Clipsey Brook.
 Infill canal to required depth to provide cover above new culvert to Clipsey Brook.
 Line with HDPE and protect with concrete/armater.
 Build gabions in front of masonry walls.
 Option, use volunteer labour to rebuild original walls.

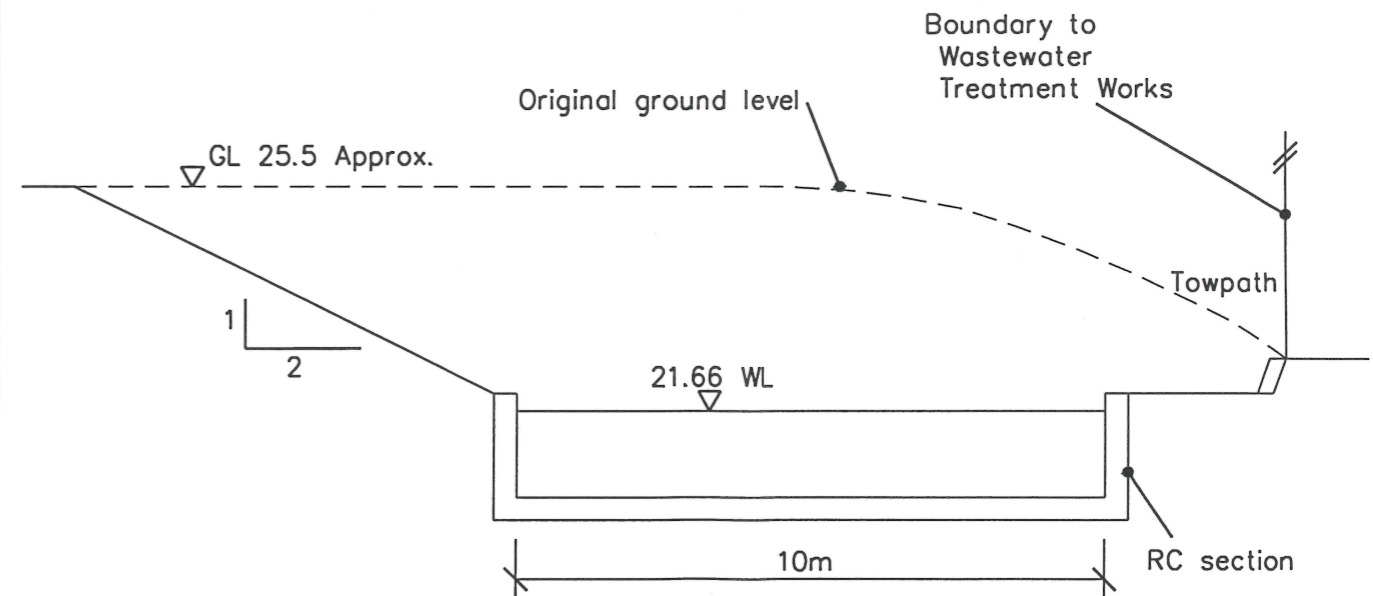
SECTION No. 10

OLD DOUBLE LOCK TO BOARDMANS LANE

(CH 20,695-21,090m)

DESCRIPTION - Provide channel on new alignment adjacent to St. Helens Sewage Treatment Works. Channel will be reinforced concrete lined. Informal footpath to be provided on non-towpath side.

RESTORED SECTION -



RESTORATION - Excavate channel in inert fill, and construct R.C. channel.

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Drawing Number **AY2311/120/850/055**

A3

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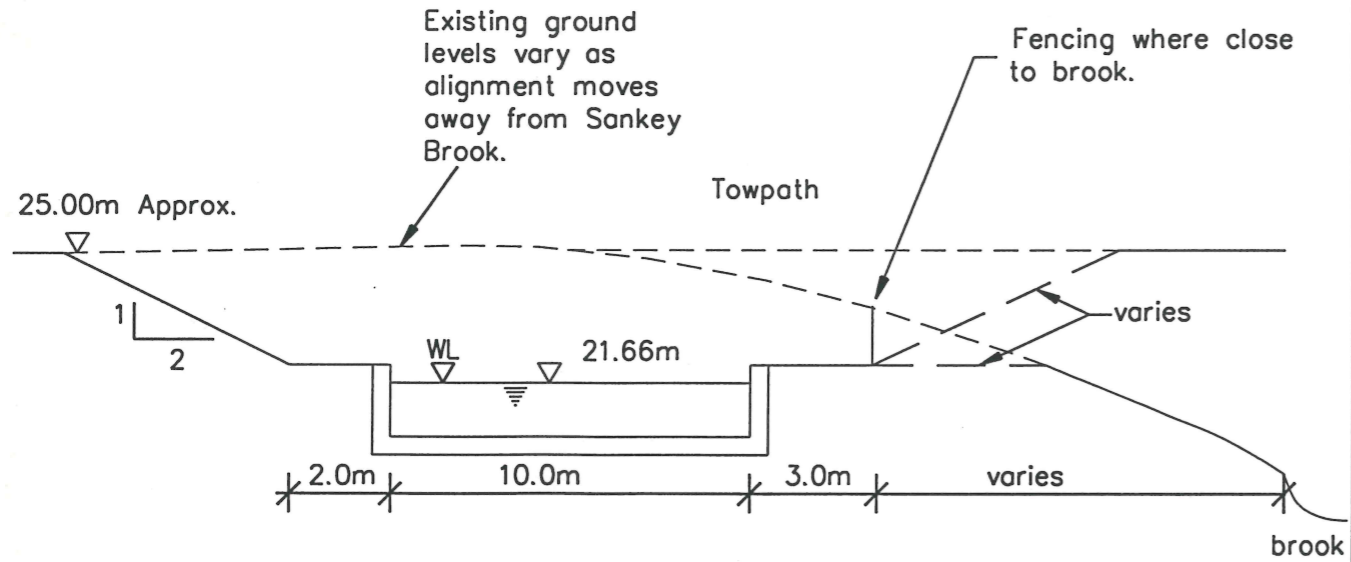
SECTION No. 11

BOARDMANS LANE TO PARK ROAD LOCK

(CH 21,090-21,525m)

DESCRIPTION - New alignment mostly through filled ground adjacent to Sankey Brook

RESTORED SECTION -



RESTORATION - Excavate new channel through inert fill. Construct reinforced concrete channel. Channel width reduced to 5.5m in restricted corridor adjacent to Boardmans Lane.

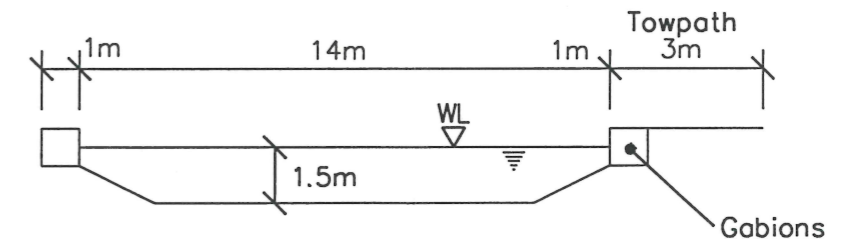
SECTION No. 12

BUILDERS YARD TO PARR STREET BRIDGE

(CH 23,670-23,800m)

DESCRIPTION - Reinstatement of canal with St. Helens. Provide mooring both sides to access local facilities and utilize minimum two way working width. It is possible that this section could be paid for by a developer. If a sympathetic development is proposed more expensive materials may be used to make this a feature.

RESTORED SECTION -



RESTORATION - Excavate channel to 1.5m deep. Line with HDPE and protect with concrete/armater. Install gabions.

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Drawing Number **AY2311 / 120 / 850 / 056**

A3

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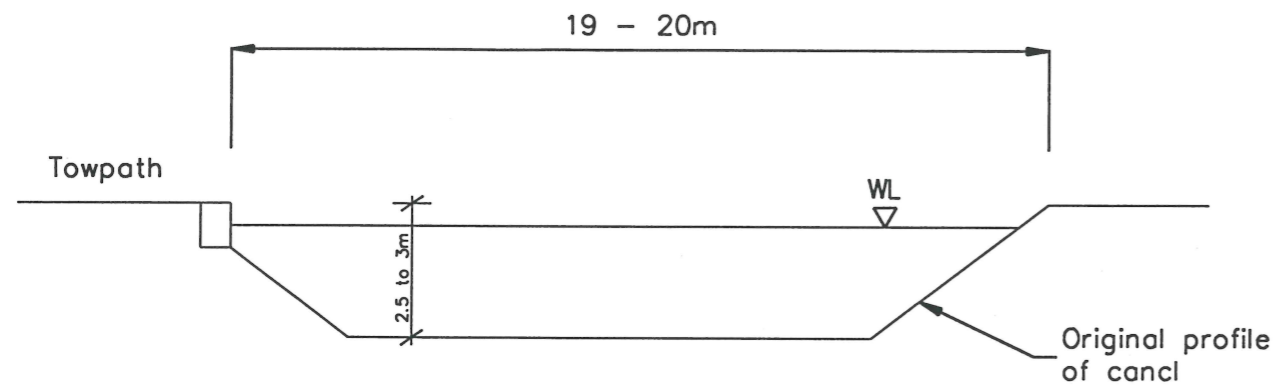
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SECTION No. 15

BEWSEY LOCK TO SANKEY BROOK OVERFLOW (CH 10,460-11,060m)

DESCRIPTION - Former canal channel now dry.

RESTORED SECTION -



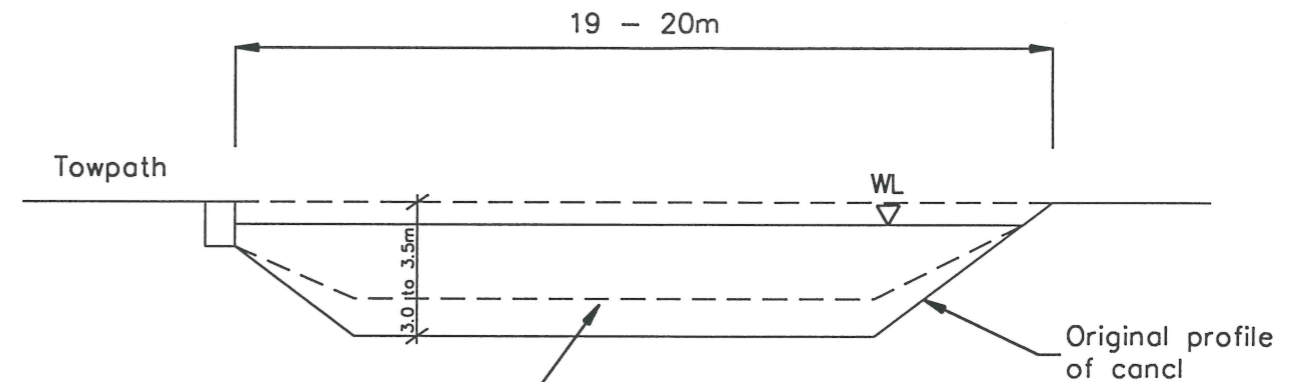
RESTORATION - Remove vegetation and organic material from channel. Reform/regrade channel and banks as necessary. Line with HDPE and protect with concrete/armater. Install gabion wall to form moorings on one side.

SECTION No. 16

HULME LOCK (OLD) TO WINWICK QUAY M62 TO WINWICK LOCK (CH 12,485-12,950m)

DESCRIPTION - Canal infilled with domestic refuse.

RESTORED SECTION -



RESTORATION - All refuse and contaminated silt to be removed and disposed of at a licensed site. Line excavation with HDPE and protect with concrete/armater. Install gabion wall to provide moorings on one side.

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Drawing Number
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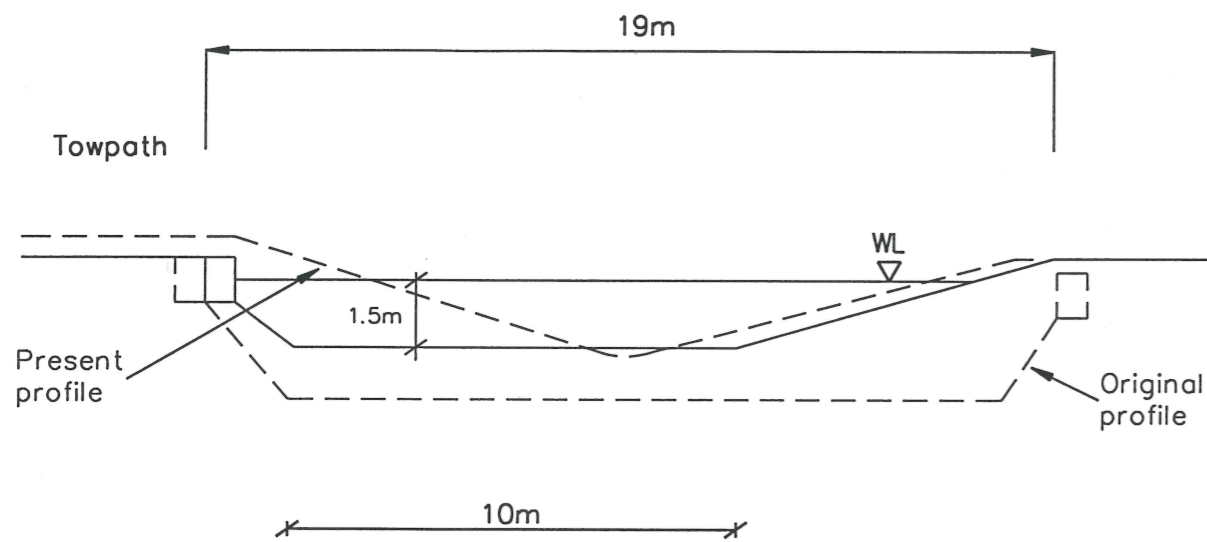
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SECTION No. 17

PARK ROAD LOCK TO MERTON BANK ROAD (CH 21,525-22,425m)

DESCRIPTION - 'In-water' section, channel reshaped to convey Rainford Brook.

RESTORED SECTION -



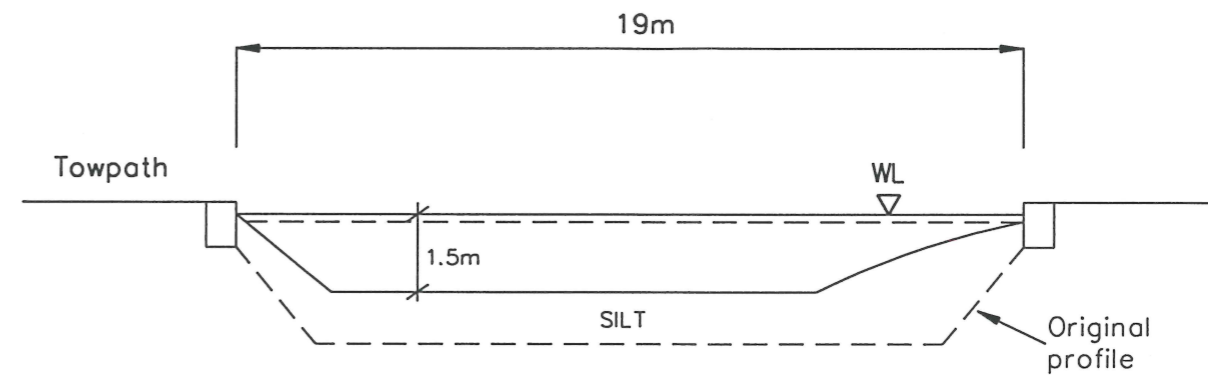
RESTORATION - Excavate to recreate navigable channel. Condition of existing masonry walls unknown, place gabions to form towpath edge. "Natural" edge to opposite bank.

SECTION No. 18

MERTON BANK ROAD TO NEW DOUBLE LOCK GERARDS BRIDGE BRANCH (CH 22,425-22,750m)
(CH GB000-GB350m)

DESCRIPTION - "In-water" section, Heavily silted, conveying Rainford Brook.

RESTORED SECTION -



RESTORATION - Excavate to recreate navigable channel. Retain silt in front of existing masonry walls for benefit of marginal vegetation. Possibility of silt removal by dredging.

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Drawing Number
AY2311 / 120 / 850 / 059

A3

Purpose of issue	Auth	Date	Rev	Description	By	Date	Chk'd	Auth

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